

Introduction

High Speed Two (HS2) is the new high-speed railway for Britain.

The map on the right shows the North Chilterns area of HS2 construction being built by civils contractor, EKFB

EKFB (Eiffage, Kier, Ferrovial, BAM) have been appointed by HS2 to deliver civil engineering works across an 80km section of the railway between Warwickshire and the Chilterns

What is the current progress of the civils?

Some key statistics for our progress in the North Chilterns area can be found below:



Earthworks – 65%



Concrete viaducts 63%



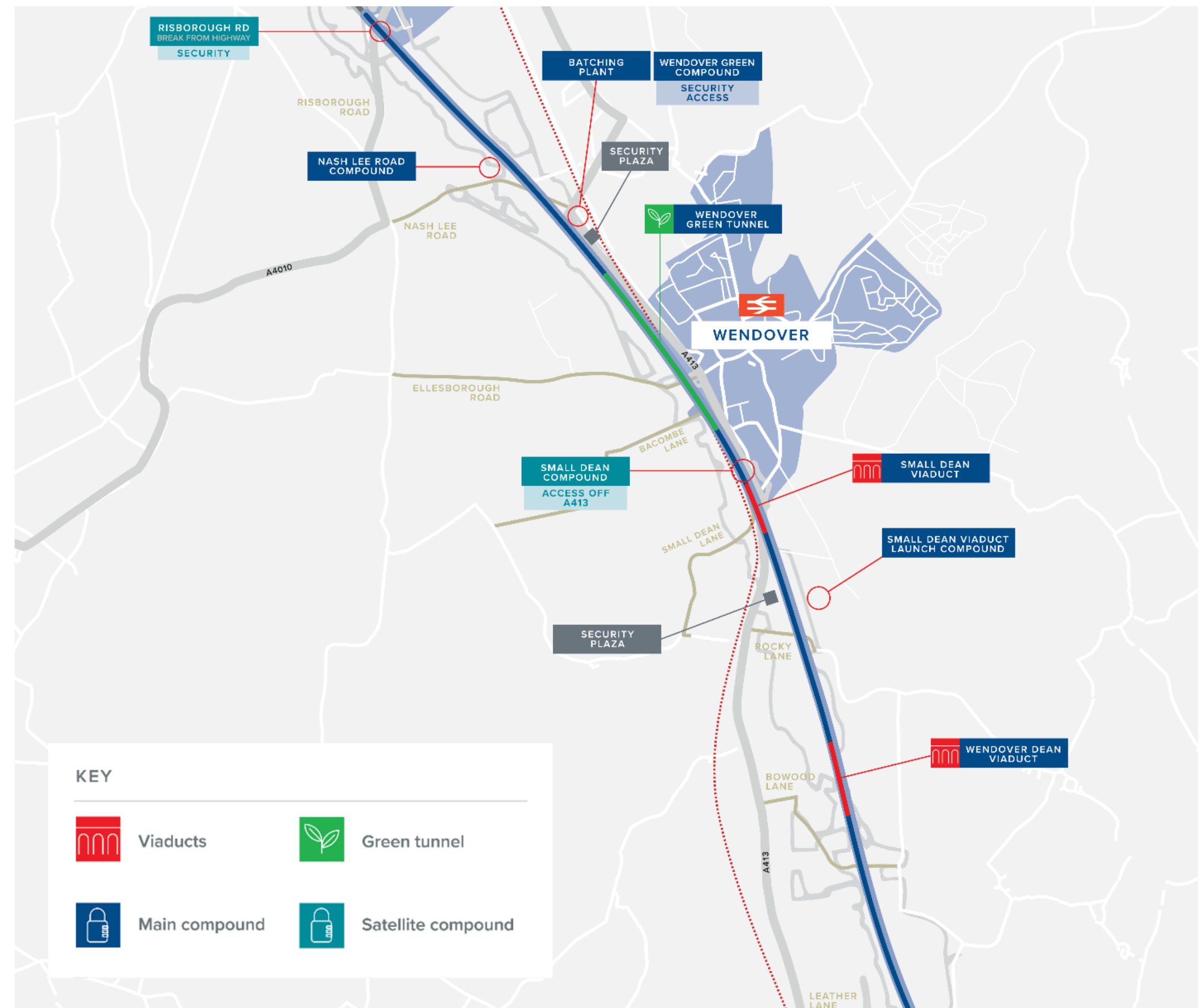
Concrete 71%



Green tunnel 27%

What happens after the main civils?

- Civils construction by EKFB is set to be completed by around the end of 2028 with landscaping throughout 2029*
- Following the completion of works, the rail systems contractor will mark the next stage of the project to transform the civils infrastructure into an operational railway



Wendover north cutting and Nash Lee

Progress through 2025

- Low permeability wall works which help to manage groundwater and reduce flood risk were completed in September 2025
- Construction of the wall began in August 2024 to minimise ingress of groundwater into cutting
- From July to August 2025, the final section of the wall was installed beneath Nash Lee Road over a four-week period
- Nash Lee Road reopened on time in mid-August, following excavation, slab installation and resurfacing
- Progress on the Nash Lee overbridge has continued steadily throughout the year.
- The steel beams of the bridge were delivered in summer and have now been installed

Lookahead

- The north cutting is being excavated and is set to continue until the end of 2027
- Nash Lee overbridge to be lowered onto its permanent bearings in Winter 2025
- The realignment of Nash Lee Road as we install the overbridge for public use:
 - 24-hour closure of Nash Lee Road (Q4 2026, dates TBC)



Nash Lee overbridge being lowered onto permanent bearings, November 2025

Wendover green tunnel

Progress through 2025

- Foundation works began at the start of 2025 for the Wendover green tunnel, a 1,185m (0.73 mile) cut-and-cover structure
- Following early foundation works, segment installation has progressed throughout the year, forming the structure of the tunnel
- The first phase of the tunnel installation has been completed, and backfill began in Autumn 2025
- Other works are ongoing across site include waterproofing, excavation, and drainage.

Lookahead

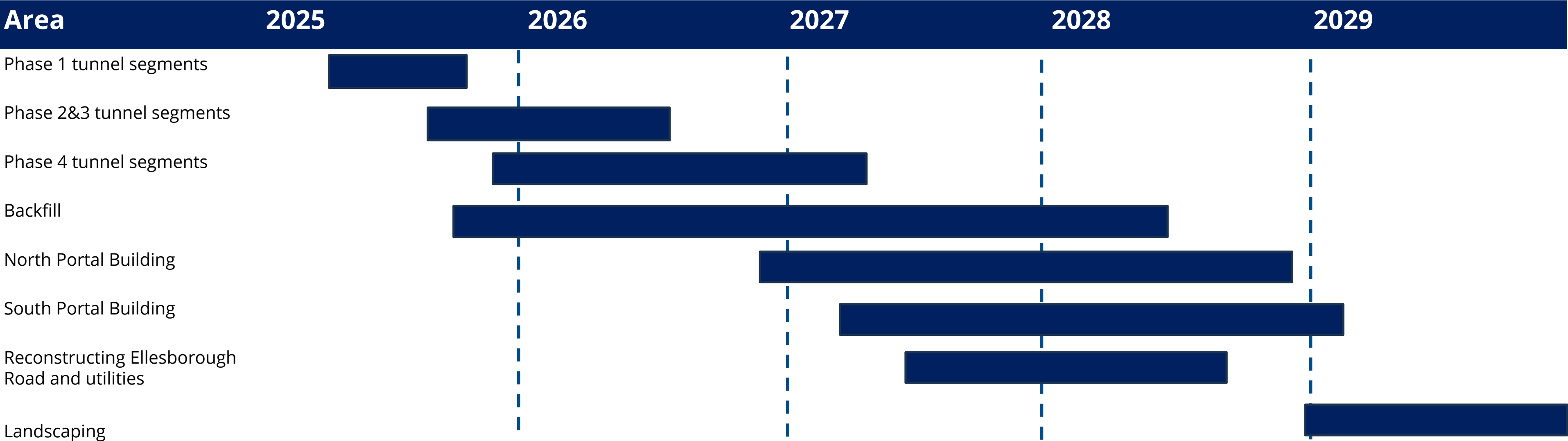
- Segmental ring installation and other associated construction is set to continue through 2026
- Construction of the foundations for the portal buildings are set to commence at the end of 2026
- The existing access route into the site will be changed due to the excavation in the Wendover north cutting area. A new site access is currently being constructed, which will require:
 - A413 Wendover bypass – weekend working with traffic management in January and February 2026



Wendover green tunnel from Ellesborough Road, Autumn 2025

Wendover green tunnel programme

- The map on the right shows each phase of working across the Wendover green tunnel
- Tunnel segments will be installed across the four phases, with portal buildings to be constructed at the north and south ends
- The table below highlights the construction programme over the next four years*
- As construction concludes toward the end of 2028 - early 2029, the landscaping of the area will commence



*Please note, dates are accurate at time of print but may be subject to change through regular programme updates.

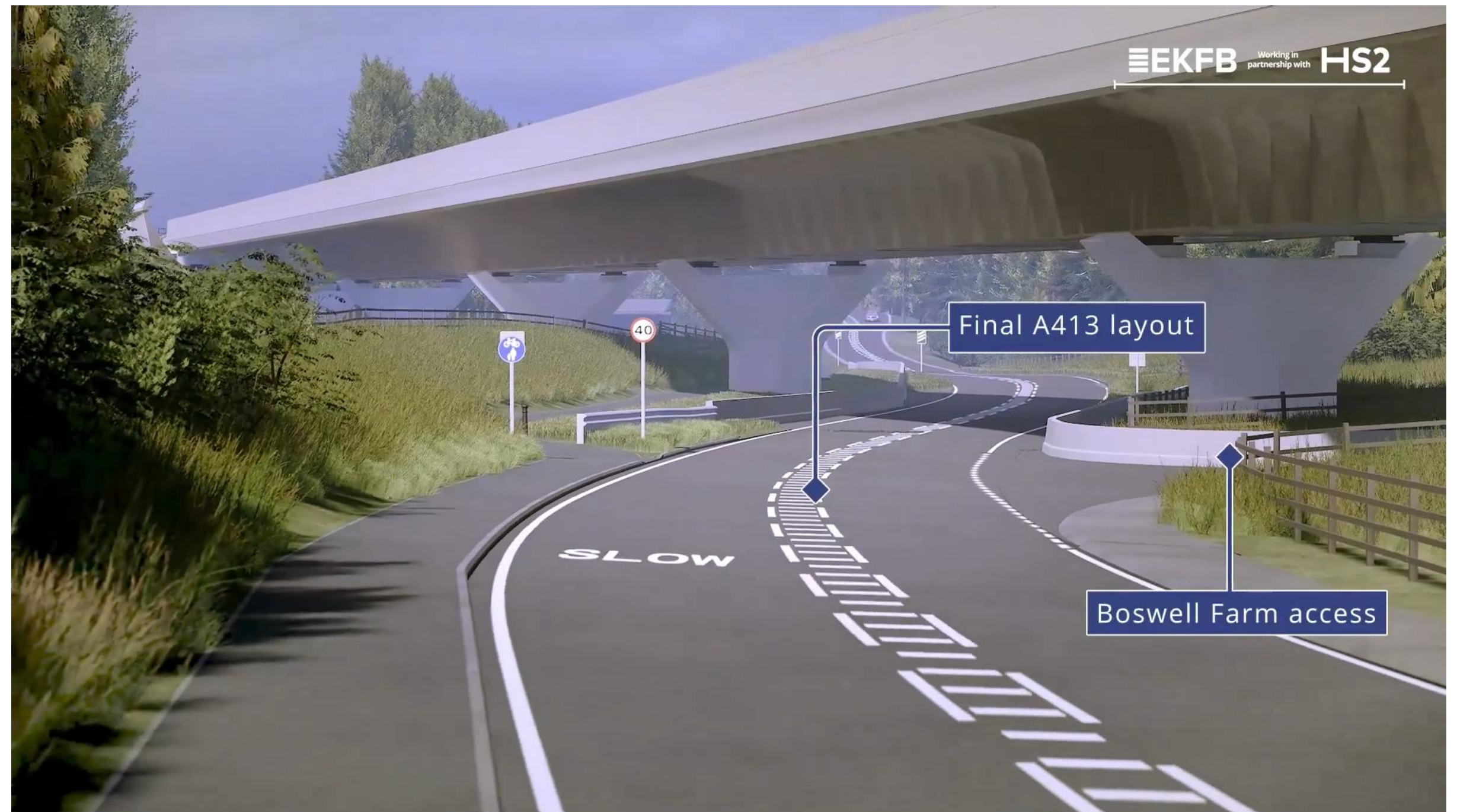
A413 London road

Progress since 2023

- We began construction on the Small Dean viaduct in 2023 where we completed the first realignment of the A413 London Road to enable the construction of the first few piers close to the railway
- In Summer 2024, the road was realigned for a second time to construct the final piers of the viaduct
- The road was closed for 24-hours at the beginning of 2025 as the viaduct deck slide was completed across the full width of the road
- In Autumn 2025, the northwest rail embankment was reinstated in preparation for the final A413 realignment.

Lookahead

- The A413 will be undergo its final realignment in Autumn 2026:
 - We anticipate the A413 London Road overnight closures to be in Autumn 2026 (dates TBC)



CGI animated view of Small Dean viaduct and A413 final layout



The A413 realignment and Small Dean viaduct construction phase video can be accessed by scanning the QR code on the left.

Small Dean viaduct

Progress through 2025

- HS2's longest and heaviest single-stage deck slide was successfully completed in February 2025, moving a 4,500-tonne structure into place over four days using a precision cable system
- From February to April 2025, the viaduct deck was lowered onto its permanent bearings
- Since the summer, concrete deck pours have been undertaken in stages, as well as work on abutments, slabs, and walls

Lookahead

- Parapets are set to be installed on the viaduct from December 2025, this will require:
 - Railway possession from 25 – 29 December 2025
 - A413 London Road overnight closures in January 2026 (dates TBC)
- Small Dean south embankment works and completion of the viaduct between 2026 - 2027



Small Dean viaduct during the deck slide in January 2025



The video for the Small Dean viaduct deck slide from earlier this year can be accessed by scanning the QR code on the left

Grove Farm underbridge

Progress through 2025

- Pile construction of the top-down structure began at the beginning of the year
- Following the piling, concrete pouring and construction of the bridge deck were completed in spring
- From spring to summer, works involved deck blinding and excavation under the bridge
- The wingwalls have been under construction since September, with the structure approaching completion toward early 2026

Lookahead

- General works ongoing until completion of the underbridge in early 2026
- Small Dean north embankment and earthworks to continue until 2027
- Small Dean Lane and temporary bridleway closure set to continue into 2027, due to ongoing construction and heavy machinery in the area
- Misbourne Greenway link to Wendover to be delivered upon completion of HS2 construction works



Grove Farm underbridge, November 2025

Wendover Dean viaduct

Progress through 2025

- At the beginning of the year, the permanent bearings were installed to replace the temporary bearings, which enabled completion of the lowering process of the steel viaduct onto the bearing
- Work on the lower deck progressed well, with concrete poured in a staged sequence on the precast planks at this level
- Precast planks on the top deck that were temporarily removed for access to construct the lower-level concrete slabs have been reinstated to enable construction works on the top deck to commence
- The top deck concrete deck pours are currently underway, with seven concrete pours completed and 12 more scheduled as part of the ongoing build

Lookahead

- Viaduct construction to continue until 2027:
 - Deck, slabs, walls
 - North and south abutments
 - Parapet installations
 - North and south embankments



Wendover Dean viaduct, Autumn 2025

Rocky Lane realignment and underbridge

Progress through 2025

- In February 2025, Rocky Lane was closed for a 6-month period to construct the realignment of the road through the underbridge
- During the closure, we carried out earthworks, drainage, utility work, construction of abutments, wing walls, and the deck of the bridge
- The milestone of completing the road realignment in six months was achieved. The road was reopened on its new alignment via the underbridge, in August 2025

Lookahead

- Rocky Lane will remain open while Leather Lane is closed for overbridge construction
- A daytime lane closure to install the permanent signage – currently planned for December 2025 (date TBC)
- Rocky Lane culvert installation late 2025 and early 2026
- Embankment works on both sides of the structure in 2026
- Parapet installation on the bridge deck in 2026 (date TBC)
- Bridge deck works, including waterproofing to be completed



Rocky Lane following the opening on the new alignment, August 2025

HS2 in your area

What is HS2

HS2 is Britain's new high-speed railway that supports the government's number one mission: growth. When it's fully operational, the line will connect London and Birmingham in just 49 minutes. This will create better journeys for passengers.

Building the railway

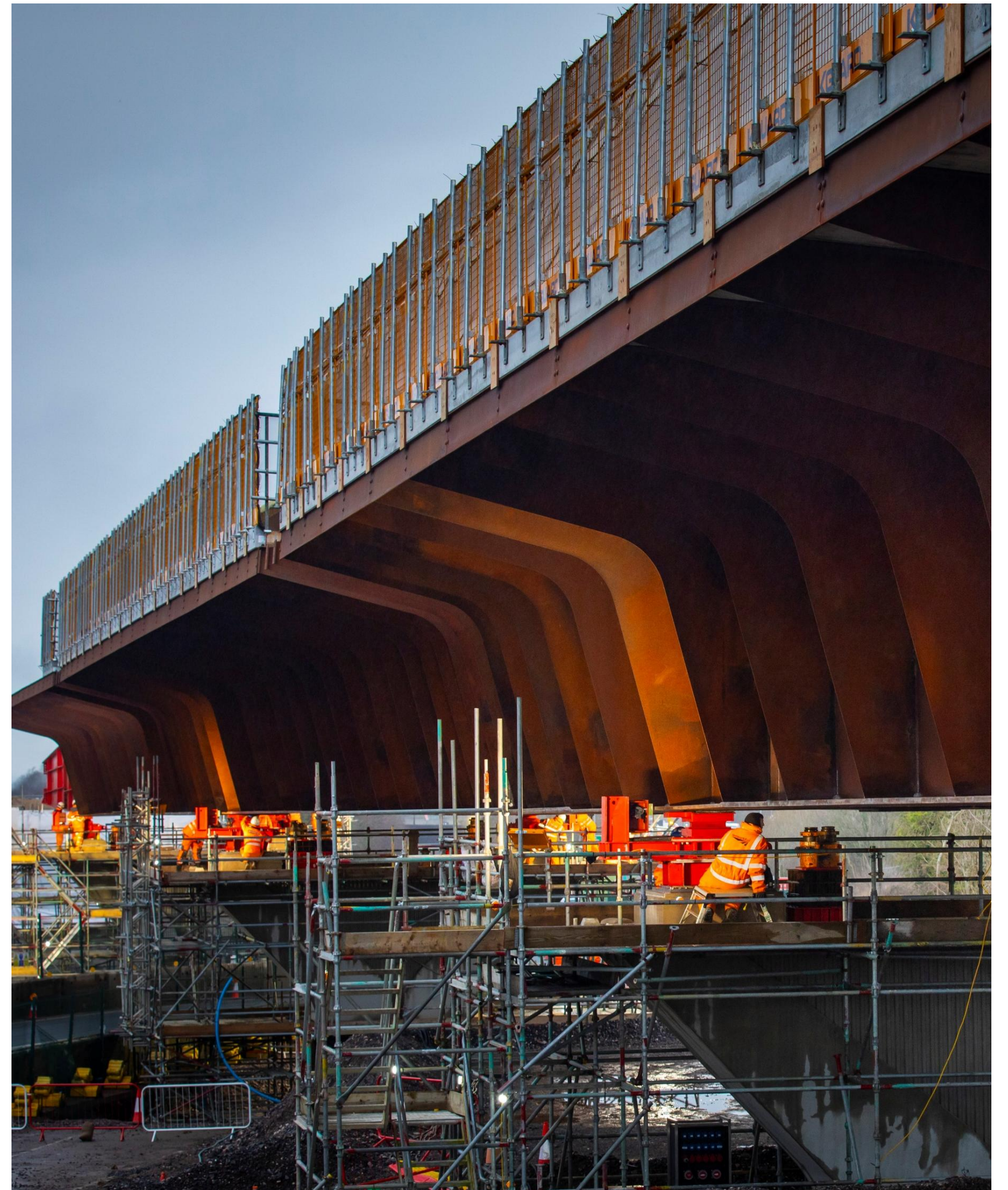
We're making progress building the main infrastructure for the new line. We've completed 80% of tunnelling and are working on major bridges, viaducts and embankments, and HS2's new stations.

- Next, we'll install the rail systems – track, power and signalling – to make the railway operational.
- HS2 is the biggest upgrade to the network in over 100 years. We're working with communities along the route to minimise disruption during construction.

Reset

Throughout 2025 we have been resetting the HS2 programme to break the cycle of cost increases and schedule delays.

- The reset will provide a realistic and achievable cost and schedule and put activity across the route back into the correct sequence.
- We will provide government with new cost and schedule ranges at the end of the year and begin building a new baseline for approval. This will set out a timeline to complete HS2. With that timeline we can have more certainty in our plans and we will be able to provide communities, like Wendover, a clear view of our programme to day one of the trainline being operational.



Small Dean viaduct during the deck slide in January 2025



Test area

A driving force behind HS2's revised scheduling plan is the completion of an initial section of the HS2 route where high-speed trains will be tested - the **'test area'**

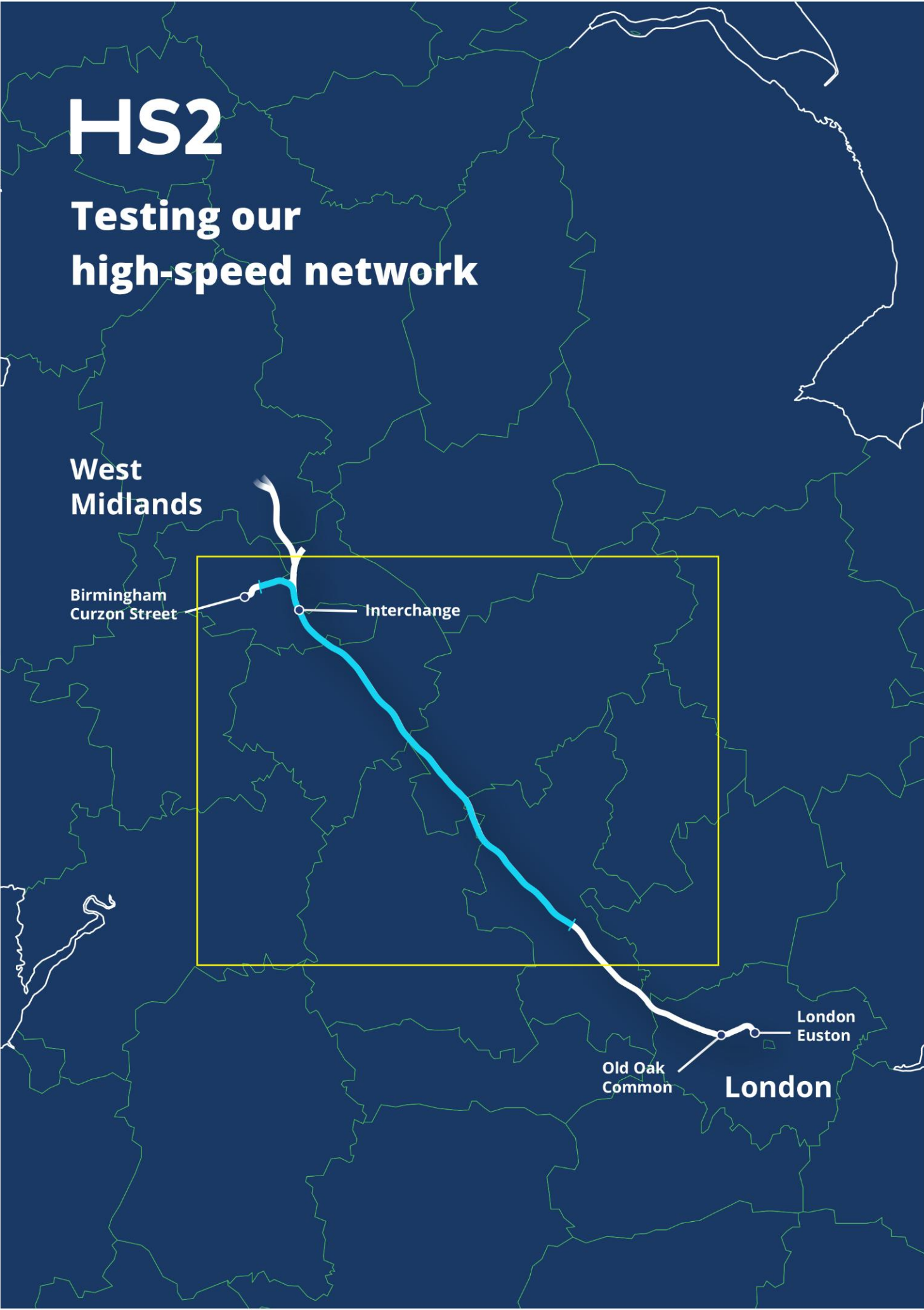
- A review as part of reset has concluded that insufficient time had been allocated to safely complete the railway's multiple testing stages.
- The test area will now be delivered in phases, allowing testing to start earlier and ensuring the programme remains on schedule.
- The first section is approx. 50 miles long and runs from Washwood Heath in Birmingham to the north portal of the Wendover green tunnel in Buckinghamshire.
- This section of the trace will be overlaid with track, power, signalling and communications systems ready for testing to begin.

The completion of civil engineering works on this section of the route is now the overriding priority, ready for Rail Systems.

Alongside the need to operate within the annual funding envelope from government, it is why decisions have been made to defer works elsewhere on the route.

For Wendover, this means that we will continue to keep building and progressing with the programme to ensure that we are able to fully deliver the critical pathway for the HS2 test area.

HS2 Test area



Working in partnership with

